

GENERAL NOTES

- 1.) ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION ACTIVITY PLAN.
- 2.) ALL CONSTRUCTION TRAFFIC OPERATING ON OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN IN RADIO CONTACT WITH AIR TRAFFIC AT ALL TIMES (FREQUENCY 122.8). THE CONTRACTOR SHALL PROVIDE HIS OWN RADIOS.
- 3.) REFER TO THE CONSTRUCTION ACTIVITY PLAN AND THE SPECIFICATIONS FOR REQUIREMENTS CONCERNING COORDINATION OF CONSTRUCTION ACTIVITIES.
- 4.) WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT DIRECTOR.
- 5.) THE CONTRACTOR SHALL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLANS.
- 6.) BROKEN CONCRETE, BROKEN ASPHALT AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF BY THE CONTRACTOR OFF THE AIRPORT PROPERTY.
- 7.) VEHICLES AND EQUIPMENT SHALL NOT BE ALLOWED WITHIN 65' OF AN ACTIVE TAXIWAY CENTERLINE AND 200' OF AN ACTIVE RUNWAY CENTERLINE.
- 8.) ALL PAVEMENTS, DRIVES AND ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED IN KIND BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT DIRECTOR. NO ADDITIONAL COMPENSATION SHALL BE MADE TO THE CONTRACTOR FOR THIS WORK.
- 9.) EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION TO THE ENGINEER AND THE AIRPORT DIRECTOR.
- 10.) THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS ON A DAILY BASIS WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF THE ENGINEER.
- 11.) THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.
IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO NOTIFY 1-800-DIG-RITE AND DETERMINE THE ACTUAL LOCATION OF ALL SUCH UTILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES AND F.A.A. UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND F.A.A. OF HIS/HER OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, F.A.A., THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTORS OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.
- 12.) THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT IN CONFORMANCE WITH AC 150/5370-2 OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION ACTIVITY.
- 13.) ALL CONSTRUCTION EQUIPMENT AND VEHICLES SHALL HAVE ORANGE AND WHITE AVIATION FLAGS ATTACHED PER AC 150/5210-5B AND BE LABELED WITH COMPANY LOGO.
- 14.) THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS TO NOT PENETRATE THE FAA PART 77 (AIRSPACE) SURFACES.
- 15.) THE CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO MINIMIZE DUST TO THE SATISFACTION OF THE AIRPORT AND ENGINEER. AT A MINIMUM, A WATER TRUCK SHALL BE AVAILABLE. ALL COSTS ASSOCIATED WITH DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.
- 16.) IF DURING CONSTRUCTION AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- 17.) CONSTRUCTION ACTIVITIES SHALL CONFORM TO THE GUIDELINES PRESENTED IN FAA ADVISORY CIRCULAR AC 150/5370-2E.
- 18.) IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911. ONCE THE 911 EMERGENCY AGENCIES HAVE BEEN CONTACTED, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE AIRPORT MANAGER WHO WILL NOTIFY OTHER AND/OR ADDITIONAL AUTHORITIES AS NECESSARY.
- 19.) THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE SO AS TO NOT CREATE PONDS OR STANDING WATER OR OTHER WILDLIFE ATTRACTANTS DURING THE COURSE OF THE WORK.
- 20.) THE CONTRACTOR'S CONTACT PERSON SHALL HANDLE ALL THE HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT WITH THE RELEVANT AUTHORITIES AND PURSUANT TO THE LOCAL, STATE AND FEDERAL RULES AND REGULATIONS.
- 21.) IT IS IMPERATIVE THE CONTRACTOR AND HIS PERSONNEL INCLUDING HAULING VEHICLES DILIGENTLY OBSERVE ALL THE DRIVING RESTRICTION WHILE OPERATING ON THE AIRPORT. THE AIRCRAFT ALWAYS HAVE THE RIGHT OF WAY OVER ANY VEHICULAR TRAFFIC AND CONSTRUCTION TRAFFIC MUST GIVE WAY OR YIELD TO AIRCRAFT TRAFFIC.
- 22.) THE CONTRACTOR WILL BE PENALIZED AND FINED BY THE FAA FOR RUNWAY INCURSIONS. THE CONTRACTOR CAN BE FINED BY THE FAA FOR NONCOMPLIANCE WITH AIRPORT RULES AND REGULATIONS SUCH AS NOT YIELDING TO AIRCRAFT MOVEMENTS OR COMPROMISING SAFETY OF PERSONS OR AIRCRAFT.
- 23.) A FLAGMAN WITH AN AIRPORT RADIO SHALL CONTROL THE MOVEMENT OF HAULING VEHICLES ACROSS ACTIVE PAVEMENT DURING HAULING OPERATIONS.
- 24.) NO PAVEMENT SHALL BE CLOSED WITHOUT PRIOR APPROVAL WITH THE AIRPORT. THE CONTRACTOR SHALL PROVIDE A 48 HOUR NOTICE TO THE ENGINEER PRIOR TO THE REQUIRED CLOSING TIME. A NOTAM SHALL BE ISSUED WHENEVER A PAVEMENT IS CLOSED OR CONSTRUCTION TRAFFIC IS WITHIN 200' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 65' OF AN ACTIVE TAXIWAY CENTERLINE.
- 25.) THE CONTRACTOR SHALL PROVIDE THE NAME AND TELEPHONE NUMBER OF THE PERSON TO ACT AS THE CONTRACTOR'S REPRESENTATIVE AVAILABLE 24 HOURS PER DAY SHOULD PROBLEMS PERTAINING TO THE CONSTRUCTION WORK ARISE WHICH WOULD REQUIRE THE CONTRACTOR'S IMMEDIATE ATTENTION.
- 26.) THE CONTRACTOR SHALL PROVIDE LIGHTS AND FLAGGING ON ALL EQUIPMENT IN CONFORMANCE WITH FEDERAL AVIATION ADVISORY CIRCULAR 150/5370-2E.
- 27.) THE CONTRACTOR'S ACTIVITY ON THE AIRFIELD SHALL BE LIMITED TO THE LIMITS OF CONSTRUCTION AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN DRAWINGS. BEYOND THE LIMITS OF CONSTRUCTION THE CONTRACTOR SHALL NOT HAVE ACCESS TO ANY OFF THE AIRFIELD AND AIRFIELD PAVEMENT WITHOUT APPROVAL OF THE CREVE COEUR AIRPORT IMPROVEMENT CORPORATION.
- 28.) EXTREME CAUTION SHALL BE USED WHEN WORKING NEAR EXISTING BUILDINGS, UNDERGROUND DUCTS OR UTILITIES, BURIED LINES, CABLES AND ANY OTHER EXISTING FEATURES. SHOULD ANY OF THE EXISTING FIXTURES OR INFRASTRUCTURE BE DAMAGED, IT SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT.

SITE ACCESS NOTES

- 1.) THE CONTRACTOR AND ENGINEER SHALL DOCUMENT THE CONDITION OF THE AIRPORT ENTRANCE ROAD AND OTHER AIRFIELD PAVEMENTS SPECIFICALLY THE APRON USED FOR HAULING PRIOR TO COMMENCING CONSTRUCTION.
- 2.) THE CONTRACTOR SHALL ACCESS THE TAXIWAY PROJECT SITE VIA THE AIRPORT ENTRANCE ROAD AND HAUL ROADS AS SHOWN ON THE SITE PLAN DRAWING.
- 3.) THE CONTRACTOR'S USE OF THE AIRPORT ENTRANCE ROAD AND ALL OTHER PAVEMENTS SHALL BE LIMITED TO RUBBER Tired EQUIPMENT AND VEHICLES.
- 4.) HAULING ON THE AIRPORT ENTRANCE ROAD WITH LOADED HAUL TRUCKS SHALL BE LIMITED TO DRY WEATHER CONDITIONS. THE INTENT OF IMPOSING HAULING RESTRICTIONS ON THE CONTRACTOR UNDER THE AUTHORITY OF THE CONTRACT IS TO MINIMIZE THE DAMAGE TO THE EXISTING AIRPORT ROADS BY LOADED HAULING VEHICLES. IF WET CONDITIONS OCCUR AND THE AIRPORT ENTRANCE ROAD OR OTHER PAVEMENTS BEGIN TO SHOW SIGNS OF DAMAGE UNDER HAULING TRAFFIC, THE CONTRACTOR SHALL SUSPEND HAULING OPERATIONS BY FULLY LOADED VEHICLES UNTIL DRY CONDITIONS PREVAIL. THE ENGINEER IN CONJUNCTION WITH THE OWNER SHALL HAVE FINAL AUTHORITY OVER APPROVAL TO HAUL.

PROJECT SCOPE AND DURATION

- 1.) THE SCOPE OF THE WORK SHALL BE AS SHOWN ON THE SITE PLAN. THE PROJECT DURATION FOR THE TAXIWAY REHABILITATION WORK AND DRAINAGE IMPROVEMENTS IS 40 CONSECUTIVE CALENDAR DAYS. THE PROJECT DURATION FOR THE SEALCOATING WORK ON THE APRON IS 3 CONSECUTIVE CALENDAR DAYS LIMITED TO MONDAY, TUESDAY AND WEDNESDAY.
- 2.) PAVEMENT MARKING SHALL BE REAPPLIED ON THE APRON AND TAXIWAY WITHIN 14 DAYS OF FINALIZING WORK.

REVISIONS

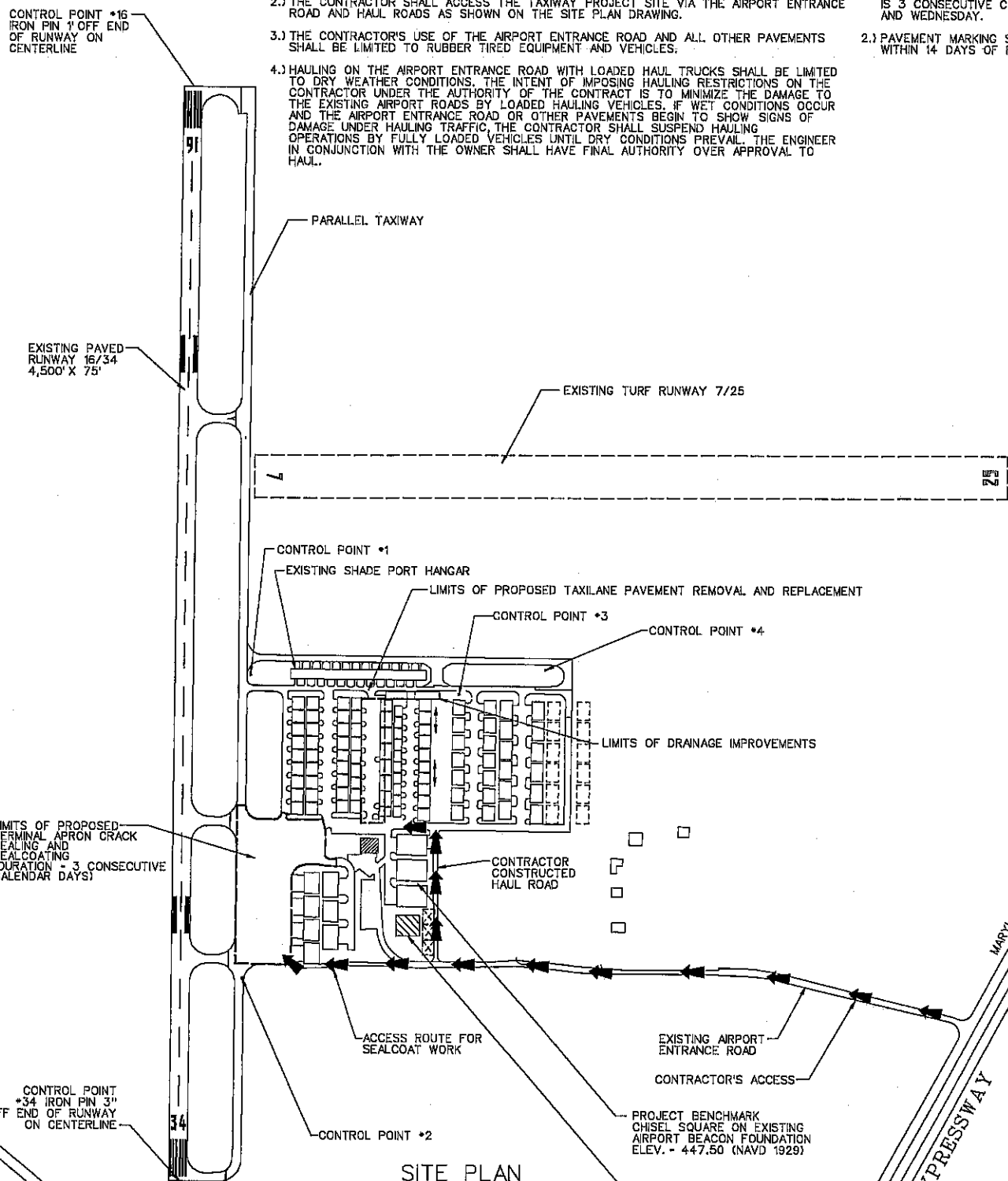
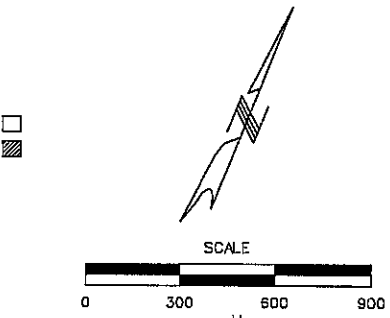
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2'
AT FULL SCALE (34X22).
PLOT 1

CREVE COEUR AIRPORT
MARYLAND HEIGHTS, MISSOURI

REHABILITATE INTERIOR
T-HANGAR TAXILANE

CMT
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CONSULTING ENGINEERS
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CONTRACTOR'S ACCESS

- 1.) CONTRACTOR'S ACCESS SHALL BE AS FOLLOWS:
 - A) THE CONTRACTOR'S ACCESS TO THE WORK SITE SHALL BE AS SHOWN ON THE SITE PLAN DRAWING.
 - B) THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGE TO AIRPORT PROPERTY CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE AIRPORT MANAGER AT NO ADDITIONAL COST TO THE AIRPORT.
 - C) ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
 - D) THE CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A WAY AS TO NOT DISTURB AGRICULTURAL AREAS OR VIOLATE THE PART 77 APPROACH AND PRIMARY SURFACES.
 - E) NO PERSONAL VEHICLES SHALL BE ALLOWED BEYOND THE CONTRACTOR'S PARKING AREA.
 - F) THE ROADS IN THE VICINITY OF THE AIRPORT MAY REQUIRE CONSTRUCTION SIGNAGE. THE CONTRACTOR SHALL PROVIDE CONSTRUCTION ENTRANCE SIGNAGE AS REQUIRED BY ST. LOUIS COUNTY HIGHWAY DEPARTMENT AND THE CITY OF MARYLAND HEIGHTS.

MAXIMUM EQUIPMENT HEIGHT: 20'
RADIO CONTROL FREQUENCY - 122.80
GEOMETRIC DESIGN DATA
DESIGN APPROACH CATEGORY: B
AIRPLANE DESIGN GROUP: I
PAVEMENT STRUCTURAL DESIGN DATA
GROSS WEIGHT - 12,500 LB.
SINGLE WHEEL MAIN GEAR

FILE: 001_Safety_Plan_01.dgn
DESIGN BY: LEW
DRAWN BY: LEW
CHECKED BY: DTN
APPROVED BY: CET
DATE: AUGUST 8, 2006
JOB No: 07425-01-06
CONSTRUCTION ACTIVITY GUIDELINES FOR CONTRACTOR PREPARED SAFETY PLAN
SHEET 1 OF 3 SHEETS

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MISSOURI STATE ROUTE 364

EARTH CITY EXPRESSWAY
MARYLAND HEIGHTS EXPRESSWAY

GENERAL PHASING CONSTRUCTION ACTIVITY NOTES:

1. PRIOR TO WORK COMMENCING, THE CONTRACTOR SHALL REQUEST CLOSURE OF THE PAVEMENT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.
2. PRIOR TO CLOSING, THE CONTRACTOR SHALL PLACE TRAFFIC CONTROL DEVICES AT THE LOCATIONS AS SHOWN ON THIS DRAWING AND/OR AS REQUIRED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL MAINTAIN THE TRAFFIC CONTROL DEVICES AS LONG AS THEY ARE REQUIRED TO BE IN PLACE.
3. AIRCRAFT TRAFFIC AND CONTRACTOR TRAFFIC SHALL REMAIN SEPARATED. CONTRACTOR IS RESPONSIBLE TO ROUTE CONSTRUCTION TRAFFIC AWAY FROM AIRCRAFT MOVEMENT AREAS AND YIELD TO ALL MOVING AIRCRAFT.
4. WHENEVER HAULING MATERIALS, THE CONTRACTOR SHALL HAVE A POWER BROOM OPERATING ON THE PROJECT AND CONTINUOUSLY CLEAN THE PAVEMENT OF ALL LOOSE PARTICLES AND DEBRIS DEPOSITED ON THE PAVEMENT BY THE CONTRACTOR VEHICLES.
5. TERMINAL APRON CRACK FILL AND SEALCOATING IS LIMITED TO ONE CONTINUOUS 48 HOUR PERIOD. THE APRON WILL NOT BE REOPENED UNTIL COMPLETELY DRY TO THE SATISFACTION OF THE ENGINEER.
6. CONSTRUCTION TRAFFIC IS NOT PERMITTED TO USE ANY EXISTING AIRFIELD PAVEMENT UNLESS SHOWN ON THIS SHEET.
7. PARALLEL TAXIWAY OPEN FOR DURATION OF PROJECT EXCEPT DURING CONTRACTOR WORKING DURING CONTINUOUS 72 HOUR PERIOD FOR TERMINAL APRON CRACK FILLING AND SEALCOAT.

WORK AREA 1 NOTES:

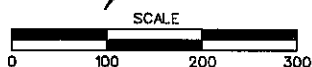
1. WORK AREA 1 INCLUDES DEMOLITION, REMOVALS, EXCAVATING, INSTALLING DRAINAGE PIPE AND INLETS, PAVING AND PAVEMENT MARKING FOR TAXILANE DE.
2. THE DURATION OF WORK AREA 1 IS 40 CONSECUTIVE CALENDAR DAYS. WORK AREA 1 MAY BE CONCURRENT WITH WORK AREA 2.
3. DURING WORKING HOURS PLACE LOW PROFILE BARRICADES AS SHOWN.
4. DURING NON-WORKING HOURS PLACE LOW PROFILE BARRICADES AS SHOWN.
5. THE CONCRETE MUST CURE A MINIMUM OF 14 DAYS PRIOR TO PAVEMENT MARKING. THE PAVEMENT MARKING CAN OCCUR AFTER THE 40 DAY WORK PERIOD FOR WORK AREA 1.
6. PAVEMENT MARKING SHALL BE PLACED IN TWO COATS.

WORK AREA 2 NOTES:

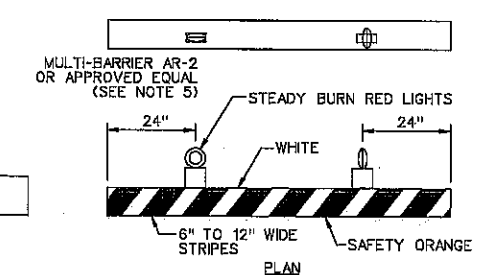
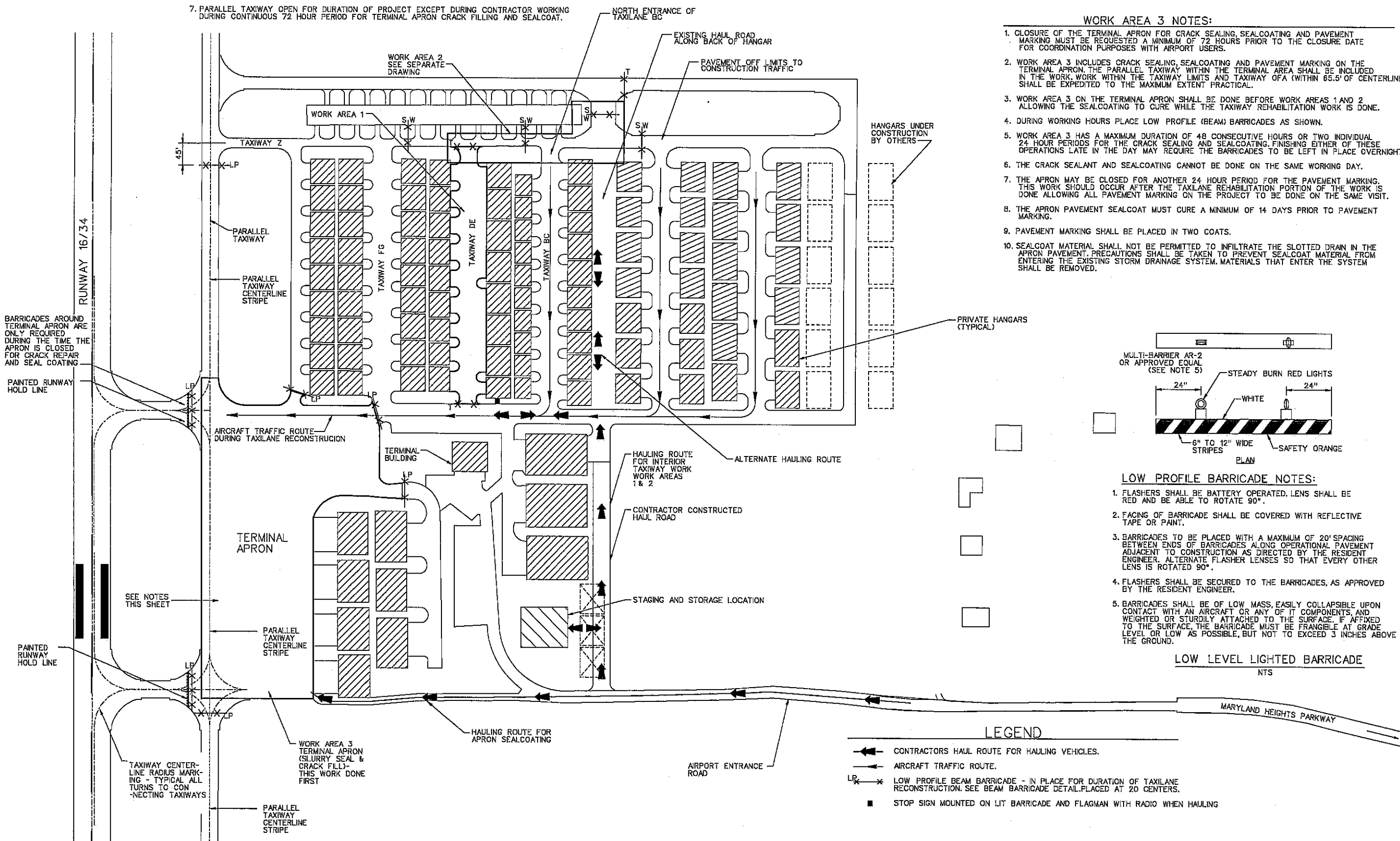
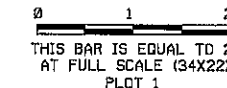
1. CLOSE TAXIWAYS AND INSTALL BARRICADES PRIOR TO INITIATING WORK AREA 2 WORK. INSTALL AND REMOVE BARRICADES ON A DAILY BASIS.
2. WORK AREA 2 INCLUDES DEMOLITION, REMOVALS, EXCAVATING AND INSTALLING ALL DRAINAGE PIPE AND INLETS ALONG THE SOUTH SIDE OF THE TAXIWAY Z. ADDITIONAL ITEMS INCLUDE PAVEMENT REPAIRS, BACKFILLING, GRADING, SEEDING AND MULCHING.
3. THE DURATION OF WORK AREA 2 IS 7 CONSECUTIVE CALENDAR DAYS. WORK AREA 2 MAY BE CONCURRENT WITH WORK AREA 1.
4. DURING WORKING HOURS PLACE LOW PROFILE (BEAM) BARRICADES AS SHOWN.
5. DURING NON-WORKING HOURS PLACE LOW PROFILE BARRICADES AS SHOWN.
6. THE NORTH ENTRANCE TO TAXILANE BC MAY BE CLOSED A MAXIMUM OF 6 NIGHTS. REPLACEMENT PAVEMENT MUST BE IN PLACE AND CURE A MINIMUM OF 24 HOURS BEFORE THE NORTH ENTRANCE TO TAXILANE BC CAN BE RE-OPENED TO TRAFFIC.
7. SEEDING DOES NOT HAVE TO OCCUR WITHIN THE 7 DAY WORK PERIOD ESTABLISHED FOR PHASE 2, BUT PERMANENT SEEDING MUST OCCUR WITHIN 21 DAYS OF COMPLETING GRADING OPERATIONS.

WORK AREA 3 NOTES:

1. CLOSURE OF THE TERMINAL APRON FOR CRACK SEALING, SEALCOATING AND PAVEMENT MARKING MUST BE REQUESTED A MINIMUM OF 72 HOURS PRIOR TO THE CLOSURE DATE FOR COORDINATION PURPOSES WITH AIRPORT USERS.
2. WORK AREA 3 INCLUDES CRACK SEALING, SEALCOATING AND PAVEMENT MARKING ON THE TERMINAL APRON, THE PARALLEL TAXIWAY WITHIN THE TERMINAL AREA SHALL BE INCLUDED IN THE WORK. WORK WITHIN THE TAXIWAY LIMITS AND TAXIWAY OFA (WITHIN 65.5' OF CENTERLINE) SHALL BE EXPEDITED TO THE MAXIMUM EXTENT PRACTICAL.
3. WORK AREA 3 ON THE TERMINAL APRON SHALL BE DONE BEFORE WORK AREAS 1 AND 2 ALLOWING THE SEALCOATING TO CURE WHILE THE TAXIWAY REHABILITATION WORK IS DONE.
4. DURING WORKING HOURS PLACE LOW PROFILE (BEAM) BARRICADES AS SHOWN.
5. WORK AREA 3 HAS A MAXIMUM DURATION OF 48 CONSECUTIVE HOURS OR TWO INDIVIDUAL 24 HOUR PERIODS FOR THE PAVEMENT MARKING. FINISHING EITHER OF THESE OPERATIONS LATE IN THE DAY MAY REQUIRE THE BARRICADES TO BE LEFT IN PLACE OVERNIGHT.
6. THE CRACK SEALANT AND SEALCOATING CANNOT BE DONE ON THE SAME WORKING DAY.
7. THE APRON MAY BE CLOSED FOR ANOTHER 24 HOUR PERIOD FOR THE PAVEMENT MARKING. THIS WORK SHOULD OCCUR AFTER THE TAXILANE REHABILITATION PORTION OF THE WORK IS DONE ALLOWING ALL PAVEMENT MARKING ON THE PROJECT TO BE DONE ON THE SAME VISIT.
8. THE APRON PAVEMENT SEALCOAT MUST CURE A MINIMUM OF 14 DAYS PRIOR TO PAVEMENT MARKING.
9. PAVEMENT MARKING SHALL BE PLACED IN TWO COATS.
10. SEALCOAT MATERIAL SHALL NOT BE PERMITTED TO INFILTRATE THE SLOTTED DRAIN IN THE APRON PAVEMENT. PRECAUTIONS SHALL BE TAKEN TO PREVENT SEALCOAT MATERIAL FROM ENTERING THE EXISTING STORM DRAINAGE SYSTEM. MATERIALS THAT ENTER THE SYSTEM SHALL BE REMOVED.



REVISIONS		
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- LOW PROFILE BARRICADE NOTES:**
1. FLASHERS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
 2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
 3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 20' SPACING BETWEEN ENDS OF BARRICADES ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
 4. FLASHERS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
 5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF ITS COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR LOW AS POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
- LOW LEVEL LIGHTED BARRICADE**
NTS

- LEGEND**
- ← CONTRACTORS HAUL ROUTE FOR HAULING VEHICLES.
 - AIRCRAFT TRAFFIC ROUTE.
 - LB * * * LOW PROFILE BEAM BARRICADE - IN PLACE FOR DURATION OF TAXILANE RECONSTRUCTION. SEE BEAM BARRICADE DETAIL. PLACED AT 20' CENTERS.
 - STOP SIGN MOUNTED ON LIT BARRICADE AND FLAGMAN WITH RADIO WHEN HAULING

CREVE COEUR AIRPORT
MARYLAND HEIGHTS, MISSOURI
REHABILITATE INTERIOR
T-HANGAR TAXILANE

GMT
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CONSULTING ENGINEERS
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FILE: 002_SAFETY_PLAN_02.dgn
DESIGN BY: DTN
DRAWN BY: LEW
CHECKED BY: DTN
APPROVED BY: CET
DATE: AUGUST 8, 2008
JOB No: 07425-01-06
CONSTRUCTION ACTIVITY GUIDELINES FOR CONTRACTOR PREPARED SAFETY PLAN
SHEET 2 OF 3 SHEETS

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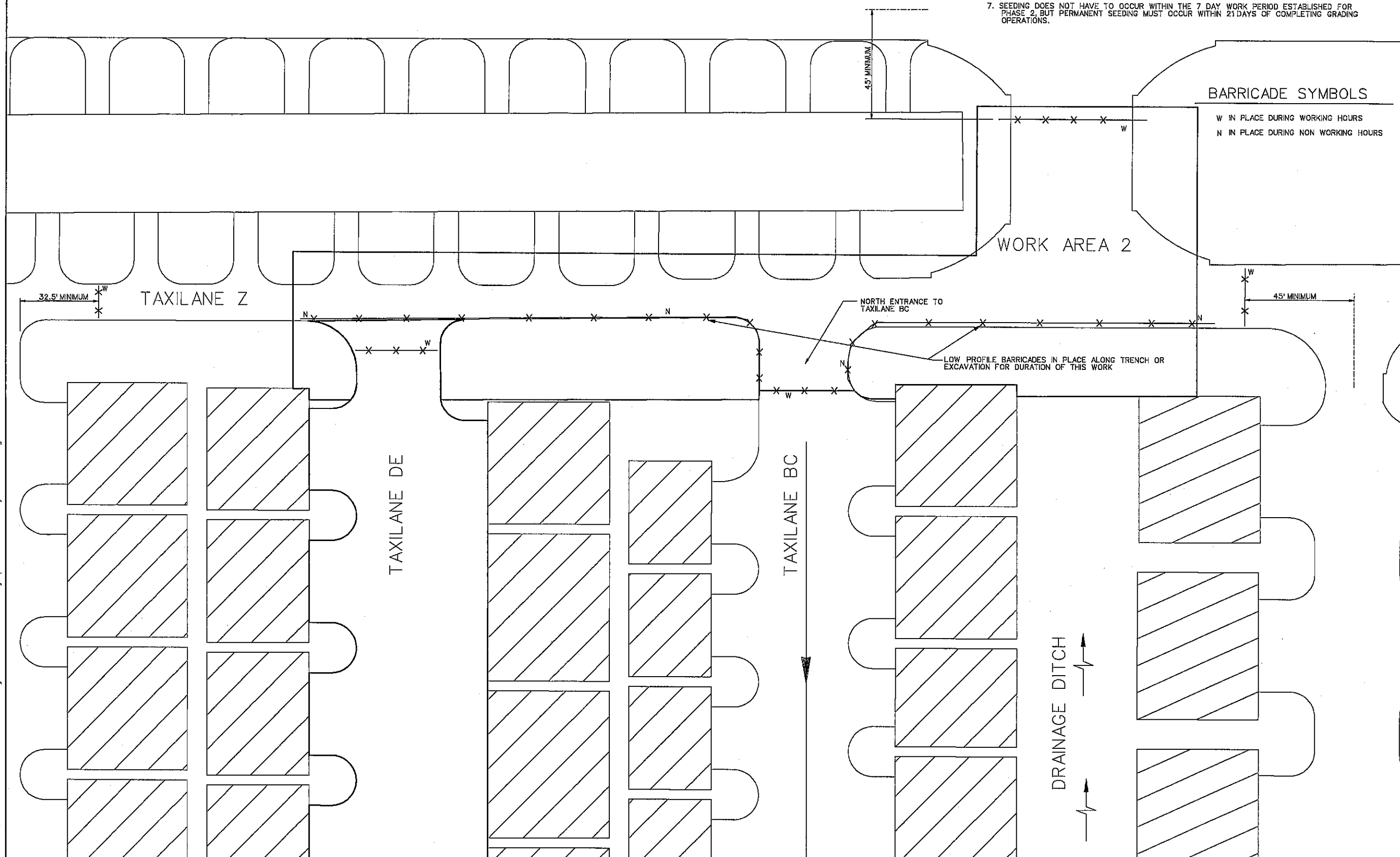
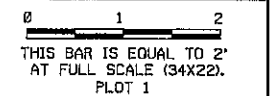
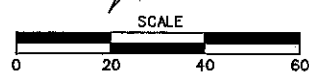
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NUMBER	BY	DATE

WORK AREA 2 NOTES:

1. CLOSE TAXIWAYS AND INSTALL BARRICADES PRIOR TO INITIATING WORK AREA 2 WORK. INSTALL AND REMOVE BARRICADES ON A DAILY BASIS.
2. WORK AREA 2 INCLUDES DEMOLITION, REMOVALS, EXCAVATING AND INSTALLING ALL DRAINAGE PIPE AND INLETS ALONG THE SOUTH SIDE OF THE TAXIWAY 2. ADDITIONAL ITEMS INCLUDE PAVEMENT REPAIRS, BACKFILLING, GRADING, SEEDING AND MULCHING.
3. THE DURATION OF WORK AREA 2 IS 7 CONSECUTIVE CALENDAR DAYS. WORK AREA 2 MAY BE CONCURRENT WITH WORK AREA 1.
4. DURING WORKING HOURS PLACE LOW PROFILE (BEAM) BARRICADES AS SHOWN.
5. DURING NON-WORKING HOURS PLACE LOW PROFILE BARRICADES AS SHOWN.
6. THE NORTH ENTRANCE TO TAXILANE BC MAY BE CLOSED A MAXIMUM OF 6 NIGHTS. REPLACEMENT PAVEMENT MUST BE IN PLACE AND CURE A MINIMUM OF 24 HOURS BEFORE THE NORTH ENTRANCE TO TAXILANE BC CAN BE RE-OPENED TO TRAFFIC.
7. SEEDING DOES NOT HAVE TO OCCUR WITHIN THE 7 DAY WORK PERIOD ESTABLISHED FOR PHASE 2, BUT PERMANENT SEEDING MUST OCCUR WITHIN 21 DAYS OF COMPLETING GRADING OPERATIONS.

LEGEND

- ← CONTRACTORS HAUL ROUTE FOR HAULING VEHICLES.
- AIRCRAFT TRAFFIC ROUTE.
- ✕ LOW PROFILE BEAM BARRICADE - IN PLACE FOR DURATION OF TAXILANE RECONSTRUCTION. SEE BEAM BARRICADE DETAIL. PLACED AT 20' CENTERS.
- STOP SIGN MOUNTED ON LIT BARRICADE AND FLAGMAN WITH RADIO WHEN HAULING



BARRICADE SYMBOLS

- W IN PLACE DURING WORKING HOURS
- N IN PLACE DURING NON WORKING HOURS

CREVE COEUR AIRPORT
MARYLAND HEIGHTS, MISSOURI
REHABILITATE INTERIOR
T-HANGAR TAXILANE



FILE: 003_Safety_Plan_03.dgn
DESIGN BY:
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